

‘BLACKWELL’S BREWERY COMPANY’



**An East Anglian Industrial Layout
set in the late 1950's to early 1960's**

**Owned and exhibited by the
Mid Essex Model Railway Club**

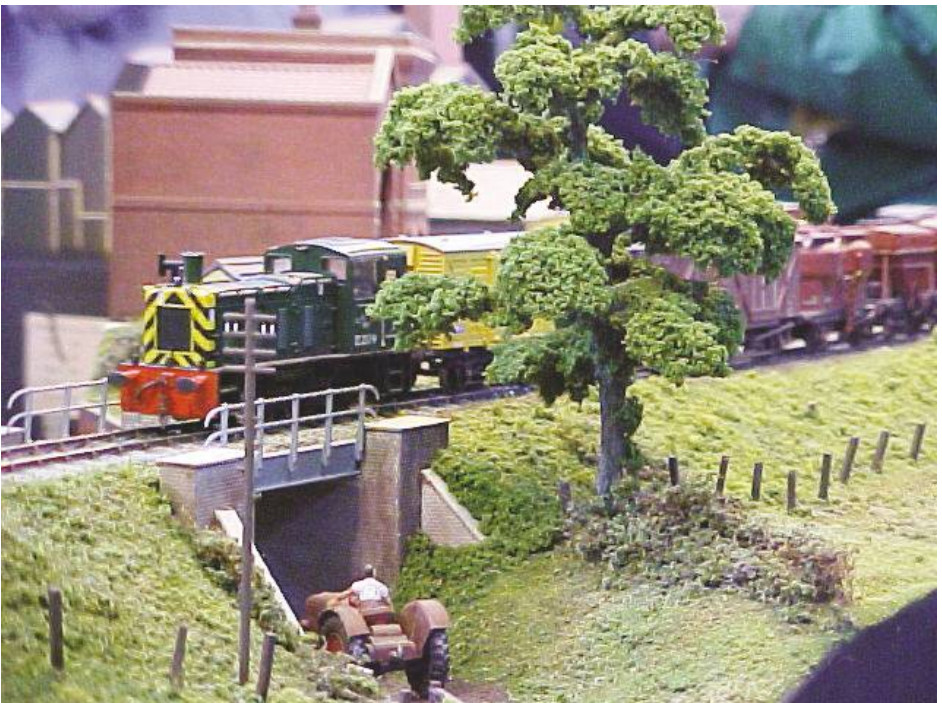
BLACKWELL'S BREWERY COMPANY

Programme Details :-

Blackwell's Brewery Company (also known as the BBC) was situated not far from Shenfield on the ex-GER cross-country route to Ongar and Dunmow. The line closed in the 1960's, a victim of Dr. Beeching, and no trace can be found of it today.

Not far from the brewery was a large brickworks whose kilns required copious amounts of coal. This coal was unloaded in the brewery yard and taken by overhead conveyor belt to the brick kilns. In return for this arrangement the modest amount of coal used by the BBC was donated by the brickworks.

The layout portrays part of the line adjacent to the brewery, together with the exchange sidings, loco shed, workmans halt and oil depot, shortly before its closure. The route was at this time being used by trains diverted because of electrification work on the Colchester main line.



The brewery buildings are based on those of an Essex brewery that was recently (2005) shut, whilst the track plan of the sidings has been devised to cause hours of headaches whilst shunting. The Brewery buildings in real life have not been demolished but turned into rather expensive apartments!

The layout boasts a lighting unit using 'older' style white and coloured spot bulbs. These bulbs are of different wattage and varying colours so as to give a more balanced effect to the overall look.

This does however mean that the operators get rather warm!

Lighting is a very important feature on any layout and as such is something that our club tries to promote to other modellers and club members.



BLACKWELL'S BREWERY COMPANY (B.B.C.)

History and Description of Layout :-

Anybody driving through central Essex will not fail to notice the numerous hamlets and villages with intensively farmed land surrounding them. As a result of the Light Railways Act of 1896 several schemes were proposed for the area to improve transport links for produce. One of these routes was an extension of the Ongar branch to Chelmsford and another was a long straggling line starting on the GER main line at Shenfield, passing through High Ongar and Dunmow, to Haverhill on the Stour Valley Line.

Our model represents the southern section of the Shenfield to Haverhill line as it was in the early 1960's. Due to the number of military airfields and fuel storage depots which were located throughout East Anglia during the Second World War, the line was rebuilt and upgraded to route availability 6, thus enabling it to be used as a diversion and relief route.

After leaving Shenfield the line curved its way across countryside before arriving at the first industry which was the Blackwells Brewery Company and adjacent Brickworks. These companies needed rail access for their raw materials and were providing another source of employment for the local inhabitants not involved in farming.

After leaving the Brewery you will notice Wyatts Green halt which was built with the line and subsequently provided a more reliable way for the workers to get to work during the Second World War.

The line then meanders past the loco facilities for the brewery shunters before making its way to High Ongar and Dunmow beyond.

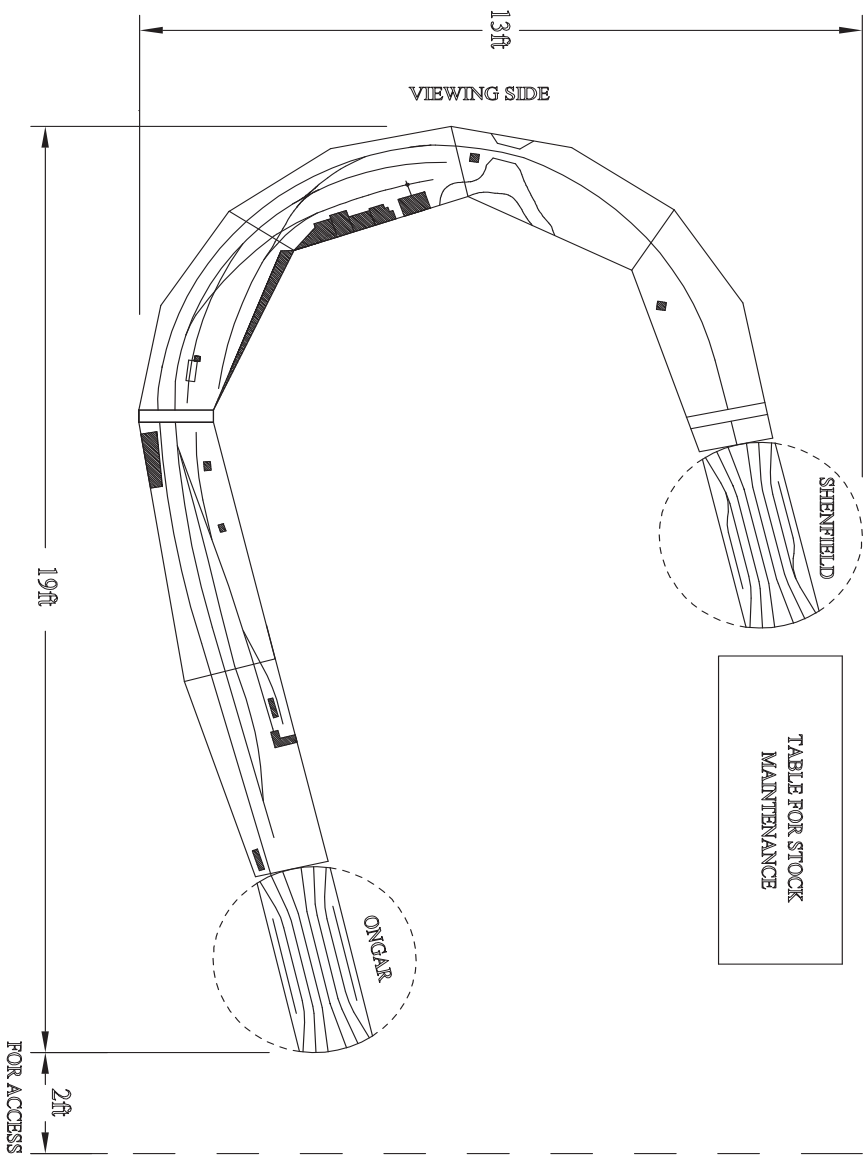


The layout has been built using a standard plywood open framed style of baseboard using EM Gauge Society pattern makers metal dowels for the alignment.

Track consists of hand made pointwork and SMP track laid on a cork base, lightly pinned and held in place with the ballast. The points are operated by Tortoise slow action point motors which we have found to be most reliable, especially when heavily used at exhibitions.

The scenery together with the hand built trees give a very good representation of the Essex countryside, (never flat) and the buildings are a mirror image of those at the Riddleys Brewery near Felstead.

The use of turntable fiddle yards at each end not only give the public a better sight of the rolling stock, but also allows us to keep trains moving without too much handling of the stock.





Rolling stock used on the layout consists of a mixture of hand and kit built models all representing the late 1950's, early 1960's transition from BR Steam to Diesel in the Eastern Region, and makes full use of the three link coupling system.

One special feature on the layout is in the Brewery where can be seen a working model of a wagon tippler. This is based on one that was situated at West Ham Power Station, now long since gone, and enables the coal to be unloaded directly into the conveyor belt and moved to the brick kilns, off scene, for burning.

Being built to EM Gauge, the layout shows what can be achieved on narrow baseboards using scale track.



Technical Details :-

Layout Size	21 ft x 14 ft
Overall Size incl. operating space	25 ft x 15 ft
Scale	4 mm
Gauge	'EM' 18.2 mm
Height	8 ft
Power	1 x 13amp socket
Layout has it's own lighting	
Consumption	700 watts / 5 amps

Requirements :-

Insurance Value (Layout & Stock)	£20,000
Layout	Free standing
Tables	One for stock (min 4x2) & two chairs
Barriers	Required
Operators	Four
Accommodation	If more than 100 miles from the Brentwood/Chelmsford area
Expenses	Petrol/Diesel for car and a van, or van hire if member's vehicle not available

Initial Contact Details :-

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