

‘BUTLER CENTRAL’



**A freelance main line with suburban
tracks in 2mm scale ‘N’ gauge,
based on the area of Bedfordshire
Northamptonshire in the
current period with BR Diesel**

**Owned and exhibited by members of the
Mid Essex Model Railway Club**

BUTLER CENTRAL:-

Butler Central as a layout plan has developed and grown from it's initial concept over the last few years.

The track layout is defined by two independent twin track circuits that have 'dumbbell' returns at each end and a large through fiddle yard to the rear. This allows for up to six trains to be continually run and 'local' trains stopping and starting on the upper level through 'Goodall Street'.

The layout is oblong in shape and can be viewed on three sides. With the fiddle yards across the back the layout is operated from the middle thereby allowing it to be positioned hard up against a wall or back to back with another layout at exhibitions.

The layout is two thirds to what could be called complete and all intentions are to finish the modelling by mid way through 2016.





Short Programme Entry :-

The track layout is defined by two independent twin track circuits that have dumbbell returns at each end and a large through fiddle yard to the rear. This allows for up to six trains to be continually run and 'local' trains stopping and starting on the upper level.

Butler Central is served by a main station building which is located above the tracks on a road bridge. A turntable is also available to engines, with a small maintenance shed co-located along with a three road long shed giving covered working conditions. There is a small goods area across the river from Butler Central, which also provides the home base for the breakdown train.

The upper level tracks are DC controlled whilst the main lower tracks and return dumbbells are DCC. A mixture of stock will be running an interesting service giving variations to the watching public.

BUTLER CENTRAL:-

Full Description of Layout and Programme Details

Butler Central has a non specific location but likely to be based on the Bedfordshire/Northamptonshire area. The main station serves the town of Butler and is called Butler Central - not due to any particular railway company but related to it's position within the townscape. Although there are two stations fairly close, the Street serves the smaller community whereas Central serves the main town, freight yards and locomotive maintenance depot.

The buildings and other structures are as generic as is feasible for the area, thereby allowing a varied era of running.

Butler Central is served by a main station building which is located above the tracks on a road over bridge. There is also a small parcels depot, and road traffic reaches the area via a slope off the over bridge. A turntable is also available to engines, with a small maintenance shed co-located along with a three road long shed giving covered working conditions. The station layout is assumed to have been altered to accomodate a change in traffic patterns, so the original island design has been supplemented by an additional platform and bay on the down side. Access is via a flight of stairs from the main building. Halfway along the island a set of freight lifts and passenger stairway has been installed to allow ease of interchange.

There is a small goods area across the river from Butler Central which also provides the home base for the breakdown train. The trackwork then enters various tunnels, above which is located part of a small town which currently is still under construction.

Exiting the fiddle yard on the far side of the layout we come out on double track main line at differing levels. The lower level tracks crossing under a girder bow spring-bridge at the site of a removed junction. Both main lines then pass under the formation of a disused line of which one bridge span remains. On the upper level a roadway exits the hill before passing on the left a small industrial site.

The upper level tracks then enter Goodall Street, named after one of our passed members, before going into another tunnel under the small townscape on the hill. This comprises of a series of buildings set in a small development which gives credence to the need for two stations.

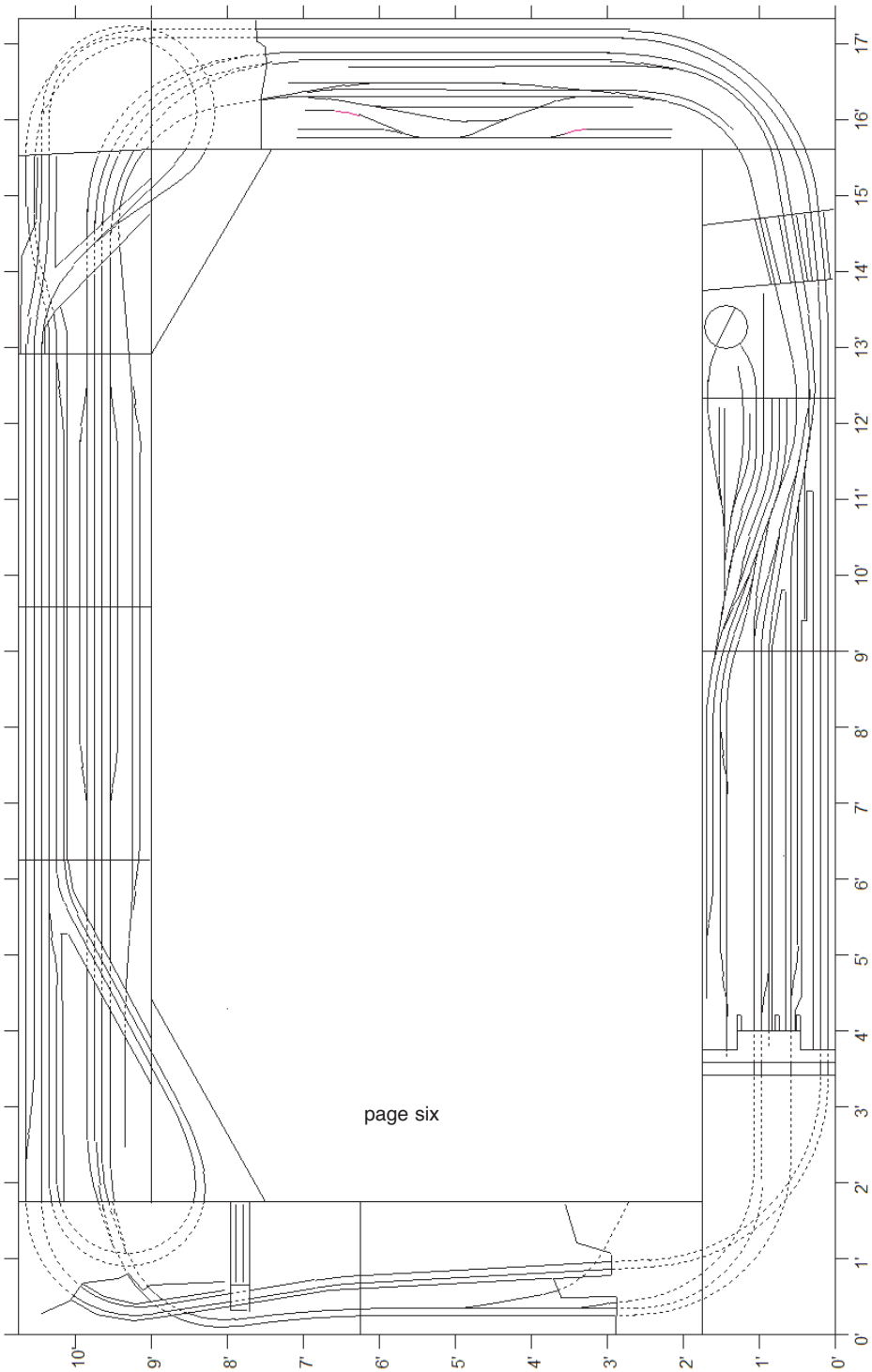
The roadway behind Goodall Street then dives under the railway and gives access to business units in the undercroft,

The lower level tracks cross a small level crossing just prior to the divergence of the track to Butler Town (phase 2), and the addition of a third track which acts as a goods line.

The layout is both DC and DCC controlled, and usually runs with the continuous tracks through the fiddle yard dumbells on DCC to keep trains running, with the lower level, yard and sheds on DC for shunting.

Being 'N' gauge it has proved challenging when trying to fit 'chips' in the locomotives for DCC running.

The layout will in time have it's own lighting unit as we are in the process of designing one that will be universal to a couple of our layouts in our club.



A layout diagram has been included in this brochure showing where all the tracks run and which ones are hidden by the various scenic scapes.

The original small layout was built by Barry Butler and on his passing donated to the club.

Many hours have since been spent in enlarging and extending the railway and as such it is still under construction.

Should you be interested in the layout for future exhibition invites then please see the reverse of this brochure for technical and contact details and / or talk to one of our club members here today.





Technical Details :-

Layout Size	20 ft x 11 ft viewed on three sides with fiddle yard across back Operated from the middle
Scale	2 mm
Gauge	'N'
Height	9 ft (when lighting unit is finished)
Power	1 x 13amp socket
Layout will have it's own lighting	
Consumption to be:-	500 watts / 5 amps

Requirements :-

Insurance Value	£ 10,000
Layout	Free standing
Tables	Two for stock
Barriers	To the three sides please
Operators	Five or Six
Accommodation	If outside M25 area
Expenses	Van hire and fuel plus fuel for 1 car

Contact Details :-

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